

PLANNING APPLICATIONS COMMITTEE
28th April 2022.

UPRN	APPLICATION NO. 21/P3428	Item No: DATE VALID 08.10.2021
Address/Site	Electrical substation adjacent to 14 Pepys Road Raynes Park SW20 8NH	
Ward:	Raynes Park	
Proposal:	Demolition of existing electrical sub-station and the erection on site of 3 x self contained flats with external amenity space and access to grounds of Langham Court	
Drawing Nos	Site location plan and drawings 1158P- 20, 200, 201, 210, 211, 212 & 213	
Contact Officer:	Leigh Harrington (020 8545 3836)	

RECOMMENDATION

Grant planning permission subject to conditions and S106 agreement.

CHECKLIST INFORMATION.

- Heads of agreement: Permit Free
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 193
- Press notice – No
- Site notice – No
- External consultations: No
- Archaeological Priority Zone – No
- Controlled Parking Zone – Yes
- Number of jobs created: N/A
- Density 277 Habitable rooms per ha.

1 INTRODUCTION

- 1.1 The application has been brought before the Planning Applications Committee for consideration as it falls outside of the Scheme of Delegation due to officer recommendation of grant permission subject to conditions and the number of objections received from local residents.

2. SITE AND SURROUNDINGS

- 2.1 The application site is a parcel of land positioned along the eastern side of Pepys Road within Raynes Park, located between no.14 Pepys Road and no. 264 Worple Road. The site sits at the junction corner, being fronted by a zebra pedestrian crossing and adjoins Langham Court at the rear. The land is narrow and elongated, running in an approximate north-west to south-east direction with an average width of 8m and length of 40m. The land currently accommodates two single storey buildings serving as an electrical substation, with a gate fronting the site restricting access. An area of hard landscaping provides access from the street to the substation at the front with some soft landscaping and a tree. The rear is made up of soft landscaping with trees and hedges.
- 2.2 The site is situated just outside of Raynes Park Town Centre, with predominantly commercial properties to the west and residential to the north and east. No. 14 Pepys Road which adjoins to the south-west is a three storey office building whilst to the north-west No. 264 Worple Road is a two storey dwellinghouse with single storey garage that is built onto the substation.
- 2.3 The site is not located within a conservation area nor is it in anyway listed. The site is located within a controlled parking zone (Zone RPE) and has a public transport accessibility level (PTAL) of 5 (0 being the lowest and 6b being the best). The site is with in a very short walk of Raynes Park mainline rail station and a number of bus services. Cycleway C31 runs nearby and links to Kingston.

3. CURRENT PROPOSAL

- 3.1 The proposals form part of a series of four applications submitted for housing on the site, LBM Ref 20/P2583 is also for consideration by members at this meeting.
- 3.2 The proposal is for demolition of existing electrical sub-station and erection on site of 3 x self-contained flats with external amenity space and access to grounds of Langham Court.
- 3.3 The new block would have an offset footprint which would adjoin with the building line of the adjacent houses on the Worple Road side of the site. The front of this square bay would be set behind landscaping whilst the other half of the front elevation would set behind a hard standing area with cycle storage and provide the entrance to the block. A pathway

would run along the Pepys Road side of the site providing a foot access to Langham Court and the refuse stores would be located along this side entrance.

- 3.4 The entrance lobby serves the stairwell to the upper flats and entrance to the ground floor flat. The ground floor flat would have a combined kitchen/living dining room area to the front with rear doors to a patio area. A centrally positioned bathroom would be provided along with a single and double bedroom with access out to a green area to the rear of the patio. That rear bedroom would feature a flat roof with a green roof arrangement.
- 3.5 At first floor level the proposals would provide a second flat, the front of which would be within a continuation of the ground floor front footprint and with a similar internal layout although this unit would be a one-bedroom unit with doors from the bedroom leading out to an amenity terrace above the ground floor element.
- 3.6 The proposed second floor would provide a similar internal layout to the first floor flat and would again be a one-bedroom unit with doors from the bedroom leading out to an amenity terrace.
- 3.7 The roof would be finished in natural or fibre cement slate for the pitched roof with a green roof to the rear and would feature a front gable with sloping roof form to the front and side.
- 3.8 The block would be finished predominantly in exposed brick work, some of which would be in the form of decorative brickwork panels. Windows would be powder coated metal or composite metal/ timber. The doors would be timber with powder coated metal louvre doors to refuse store and slated timber doors to bicycle store.
- 3.9 To soften the appearance of the rear elevation it is proposed to provide climbing plants through a combination of direct greening and indirect greening rather than a 'living wall' system. Climbing plants will be planted adjacent to the wall at ground floor level. This will be supplemented by further climbers planted on the green roof. The planting would be irrigated using rainwater from the balcony and adjacent roof area. Stainless steel wires will be used to encourage growth between first and second floor levels and across the balustrades, the textured brickwork on the facade would offer a suitable substrate for the climbers to self attach elsewhere.
- 3.10 Proposed units

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
Flat 1	2B 3P	66m ²	61m ²	32m ²	6m ²
Flat 2	1B 2P	54m ²	50m ²	6m ²	5m ²
Flat 3	1B 2p	51m ²	50m ²	6m ²	5m ²

4. **PLANNING HISTORY**

- 4.1 20/P2583 Demolition of existing substation buildings and development of site to create 2 x 3 storey 3 bedroom dwellinghouses – Currently for determination by PAC
- 4.2 20/P2682 Demolition of existing electrical sub-station and erection on site of new 3 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court - pending determination
- 4.3 21/P2784 Demolition of existing electrical sub-station and erection on site of new 4 bed dwellinghouse with front and rear gardens and access to grounds of Langham Court. - pending determination

5. **CONSULTATION**

- 5.1 Consultation letters sent to neighbours and site notice posted. Given the high level of previous objection consultation letters were sent to 193 residents. Objections were received from 113 residents raising concerns relating to;
- Fire Safety; the site is a Fire and Access service access route to the rear of the site. Fire Engines and crews would use this access.
 - Wyke Road is narrow and easily blocked and this would remove a potential access point.
 - Breach of lease agreements over pathways, gardens and accessways to and from the site.
 - Unfair to residents to build over their communal gardens for which they have a right of access.
 - The three storey height will result in loss of light and amenity to a number (10) of flats in Langham Court
 - By virtue of height and position would constitute a visually intrusive form of development
 - Loss of privacy to flats from the proposed balconies
 - The new flats will not benefit from good natural light because of the height of Langham Court.
 - It is within the green corridor and provides important wildlife habitat
 - It will remove two trees and support climate change
 - No affordable housing and just a profit for the freeholder
 - It will increase risk of flooding, floods have caused the boiler room to break down
 - The developer is just being greedy.
 - New residents would have uncontrolled access to Langham Courts gardens and refuse facilities
 - Devalue the flats and leaseholds

- The junction is to be remodelled and laid out in the future and there should be no infill development until that work is completed.

5.2 Flood Risk officers raised no objections

5.3 Transport Planning officers raised no objections

5.4 Tree Officer - The Council's Arboricultural officer commented that whilst there were no arboricultural objections seen to the scheme, the arboricultural reports should either be amended to incorporate the site visits, or attach conditions F5 & F8.

5.5 London Fire Brigade

The local fire service Station Officer undertook an initial site visit and commented that the access point is needed and would be used in the event of an incident taking place at the flats. The local Fire Station Officer Comments;

8/11/2021

"Sorry for the delay in responding. As discussed I visited the site after your visit. From the walk around that we did I would say that the access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot. I should stress however that this is my opinion and if you need official documentation this can be arranged by our fire safety department. I have contacted them and am just waiting for their reply. I would assume they would take my assessment as the brigade stance unless there is some legislation that applies. I will forward their reply to you as soon as I have it".

Officers sought further clarification as to whether the Fire Brigade Safety Department had any comments to make and the Station officer replied on

10/1/2022

I'm very sorry about the delay in getting back to you on this enquiry. I have managed at last to pin down our Fire Safety department. Unfortunately they can give no further insight into the application outside of what I said a couple of months back. This is because it is a planning application issue and as such they will

not comment on it. So from our perspective I can only reiterate what I stated before:

‘access is needed and would be used in the event of an incident taking place at the flats. Although it would be possible at the moment to manoeuvre a fire appliance down the access route to the flats it would not be something that we would do due to the lack of hard standing and the close proximity of the road. It would be necessary however to retain some form of access so personnel with equipment could access the rear of the flats on foot’.

Given the presence of other access points around Langham Court officers sought further clarification from the officer on this point and on

27/02/2022

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn’t available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access.”

5.6 UK Power Networks

The company were consulted on the proposals as they would be involved in the removal of their electricity substations. It was confirmed that the stations are operational and whilst it would be physically possible to relocate them this would involve separate applications to them.

6. POLICY CONTEXT

Relevant policies in the London Plan 2021 are;

H1 (Increasing housing supply)

H2 (Small sites)

D1 (London’s form, character and capacity for growth)

D3 (Optimising site capacity through a design lead approach)

D5 (Inclusive design)

D6 (Housing Quality and standards)

D11 (Safety & Security)

D12 (Fire safety)

GG2 (Making the best use of land)

GG4 (Delivering Homes Londoners need)

GG6 (Increasing efficiency and resilience)

SI 2 (Minimising greenhouse gas emissions)

SI.3 (Sustainable drainage)
SI. 5 (Water infrastructure)
S1.7 (Reducing waste and supporting the circular economy)
S1.8 (Waste capacity and net waste self-sufficiency)
T 5 (Cycling)
T6.1 (Residential Parking),

Merton Local Development Framework Core Strategy (2011)
CS 8 Housing choice.
CS 9 Housing targets
CS 11 Infrastructure
CS 13 Open space and nature conservation
CS 14 Design
CS 15 Climate Change
CS 17 Waste
CS 18 Transport
CS 20 Parking servicing and delivery

The relevant policies in the Council's Adopted Sites and Policies Plan 2014 are:

DM D1 Urban design and the public realm
DM D2 Design considerations
DM O2 Nature conservation, trees and hedges
DM T2 Transport impacts of development
DM T3 Car Parking and servicing standards

NPPF 2021

London Housing Supplementary Planning Guidance 2016.

7. PLANNING CONSIDERATIONS

7.1 The principal planning considerations in this case relate to the principle of residential development, fire safety, electricity supply infrastructure, design, the impact on neighbour amenity, standard of residential accommodation and trees.

7.2 Principle of development and the need for housing

7.2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.2.2 The National Planning Policy Framework 2021, London Plan 2021 policy H1 (Increasing housing supply) and the Council's Core Strategy policy CS9 all seek to increase sustainable housing provision where it can be shown that an acceptable standard of accommodation will also provide a mix of dwelling types. Policy D3 of the London Plan 2021 requires all development to make the best use of land by following a design-led

approach that optimises the capacity of sites, including site allocations. Optimising site capacity means ensuring that development is of the most appropriate form and land use for the site.

- 7.2.3 The National Planning Policy Framework and London Plan policies H1 and H2 (Small sites) promote sustainable development that encourages the development of additional dwellings at locations with good public transport accessibility. This site is within ready walking distance of Raynes Park station and a number of bus routes. Core Strategy policies CS8 & CS9 seek to encourage proposals for well-designed and conveniently located new housing that will create socially mixed and sustainable neighbourhoods through physical regeneration and effective use of space. The principle of residential development is therefore considered acceptable.
- 7.2.4 Policy H1 of the London Plan 2021 has set Merton a ten-year housing target of 9,180 new homes. By providing 3 new units the proposals would make a small but important contribution to meeting that target and providing much needed new housing. The proposal to introduce residential use to this site is considered to respond positively to London Plan and Core Strategy planning policies to increase housing supply, optimise sites and is therefore supported by Officers.

7.3 **Fire Safety**

- 7.3.1 London Plan 2021 policy D12 (Fire safety), is concerned with ensuring that new developments are not at risk from fire. It requires developments to provide suitable and convenient means of escape, and associated evacuation strategy for all, to identify suitably positioned unobstructed outside space and provide access for fire service personnel and equipment. Whilst it is acknowledged that the policy is aimed at primarily at ensuring fire safety at new developments it is considered relevant in ensuring that new developments do not impact on fire safety at other developments.
- 7.3.2 All the pending planning applications on the application site have all been accompanied by a high number of objections from the residents of Langham Court, which is a large block of flats to the rear of the site, concerned that the use of the site will hamper Fire Brigade access to them in the event of emergency.
- 7.3.4 As part of the application process, officers consulted the local London Fire Brigade Station Officer (full details in section 5.5 of this report) who, after initial reservations confirmed on the 27/02/2022 (following officer clarification in regard to the presence of other access points around Langham Court) that:

“Sorry for the delay in coming back to you. In answer to your question I don’t think that this is a fire safety issue, as confirmed by our fire safety department. The reason for the opinion I gave

was from a practical operational perspective. It could be better to have rear access to the flats for faster deployment of resources and the ability for a dual approach. However, if that wasn't available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. This would then be the same for many other places where there is only front access."

7.3.5 As set out above, the London Fire Brigade raise no fundamental objection to the proposed scheme. They confirmed that they don't think this is a fire safety issue, could be better to have rear access, however if that wasn't available the tactical plan would change but not significantly enough as to increase the risk to residents or the property. Therefore, based on the advice received, officers consider that there would be limited justification to refuse planning permission on fire safety grounds.

7.3.6 As a high number of local residents raised concerns with the very serious matter of fire safety, officers sought additional information and clarification from the applicant on this point. The applicant was therefore required to provide an independent fire strategy report with the application. Such reports are only usually required for major development however given the concerns raised by local residents, officers made a request for a fire strategy report in this instance. The applicants have submitted a Fire Strategy report compiled by R.Bosdet DMS, MIFireE (Member of the Institution of Fire Engineers) of Phoenix Executive Fire Advisory Associates that confirms that the existing access arrangements are adequate and that the proposed residential use of the site would not impact fire safety for the residents of Langham Court.

7.3.7 If planning permission is granted by members of the planning committee, it should be noted that the site of the flats at Langham Court can still be accessed along both Langham Road and Wyke Road by vehicles and pedestrians.

7.3.8 In conclusion, officers have proactively sought the views of professionals in terms of fire risk. Both the London Fire Brigade and the applicants independent fire expert have confirmed no fundamental objections or concerns with fire safety. Therefore, based on the information before officers, officers are content that the proposals would not result in an unacceptable increase in risk to the safety of Langham Court Residents in the event of fire to justify a refusal of planning permission. It should also be noted that the new buildings would be subject of separate Building Regulations approvals process which addresses fire safety matters.

7.4 **Electricity supply infrastructure**

7.4.1 Core strategy policy CS 11 (Infrastructure) states out that the Council will work with utility suppliers to ensure the safe and secure transportation of utilities such as electricity is not compromised.

7.4.2 UK Power Networks have confirmed that the substation is an active facility but have raised no objection to the application. UK Power Networks have not confirmed if the removal of the substation would result in electricity being compromised, but it is assumed that alternative facilities would need to be provided before this site was deactivated. The applicant has indicated that alternative provision could be provided within the site near to the refuse facilities associated with Langham Court in the further southwestern corner of the Langham Court land. However, this is only indicative and has not been agreed with UK Power Networks as being a feasible option.

7.4.3 In order to ensure that there are no negative impacts on local electricity supply it is recommended that a condition be attached that requires confirmation that the applicant has got the necessary approvals from UK Power Networks and that no development shall be undertaken until alternative electrical substation facilities have been provided and are operational.

7.5 **Design**

7.5.1 London Plan 2021 policy D3 (Optimising site capacity through a design lead approach), Core Strategy policy CS14 (Design) and SPP policy DM D2 (Design considerations) require developments to relate positively and appropriately to the siting, scale, proportions and massing of surrounding buildings and existing street patterns and which would enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.

7.5.2 The site has an industrial character from the functional substation structures and is located on a bend in the road between a terrace of houses along Worple Road and a more modern office block on Pepys Road. This transitional nature of the site is considered to allow for a greater diversity of design options than would be the case otherwise. The proposed design is however considered attractive and reflect the height and front gable feature of the front elevations of the adjacent houses and the materials of the offices.

7.5.3 Conditions requiring details of materials to be approved and restricting the provision of cables, wires, aerials, pipe work on the front elevation are recommended to ensure a satisfactory appearance for the development and to guard against value engineering. Subject to those factors the appearance and character of the houses are considered acceptable.

7.6 The impact on neighbour amenity

7.6.1 SPP policy DM D2 and London Plan policy D3 require that proposals do not have a negative impact on neighbour amenity from loss of light, privacy, visual intrusion or increased disturbance and that people feel comfortable with their surroundings. The proposals have generated objections on the grounds of amenity including a loss of light to neighbouring properties.

Langham Court

7.6.2 The occupiers of a number of the flats in Langham Court objected that they would suffer from a loss of light although those flats are nearly 30m from the proposed rear boundary fence. Given this distance and the orientation of the site to those flats it is considered that little weight can be afforded those objections or the building appearing visual intrusive. In terms of overlooking and impact on privacy, whilst it is acknowledged that the screening walls to the upper floor terraces would not restrict views towards Langham Court those flats are some 30m away from the proposed amenity terraces which would ensure that there is no undue loss of amenity.

284 Worple Road

7.6.3 The design of the proposed block of flats would have a staggered rear footprint. The recessed rear element of the proposed building closest to this neighbour would align with its existing garage/rear elevation. Whilst the remaining part of the proposed flats would project beyond the rear building line of this neighbouring house (6.2m at ground floor and 3.7m projection at the upper levels), this part of the proposed building and its flank wall would be inset between 2.2m and 2.7m away from the boundary. Therefore, given the level of separation from the boundary, there would remain a suitable level of breathing space from this neighbouring property.

7.6.4 It is noted that there would be some impact on light levels, given that the proposed block of flats would sit to the south of this neighbour, however as stated above, the block of flat at the upper levels only projects 3.7m beyond the rear of the neighbour and its flank wall would be inset from the boundary by at least 2.2m. It should also be noted that the existing office block (14 Pepys Road) would already cast a degree of shadowing over the neighbours garden. It is therefore considered that there would be no undue loss of light to warrant refusal of planning permission.

7.6.5 Objections were raised concerning overlooking and loss of privacy from the two upper floor amenity areas. The proposed terraces are set away from the boundary and the plans show screening walls to restrict side views over the adjoining garden at 284 Worple Road. It is therefore considered that there would be no undue overlooking or loss of privacy.

14 Pepys Road

- 7.6.6 This site is a commercial premises of offices and therefore no undue loss of amenity is anticipated.

Conclusion (amenity)

- 7.6.7 In view of these factors the proposals are not considered to be materially harmful to the amenity of neighbouring occupiers and therefore to be acceptable in this regard.

7.7 **Standard of Residential accommodation**

SPP Policy DM D2, Core Strategy 2011 policies CS 9 & CS 14 and London Plan policy D6 (Housing standards) are all policies that seek to provide additional good quality residential accommodation with safe and secure access that meets minimum internal and external space standards.

7.7.1 Accommodation schedule

Unit	Type	Prop.GIA	Req. GIA	Prop amenity	Req Amenity
Flat 1	2B 3P	66m ²	61m ²	32m ²	6m ²
Flat 2	1B 2P	54m ²	50m ²	6m ²	5m ²
Flat 3	1B 2p	51m ²	50m ²	6m ²	5m ²

- 7.7.2 All three flats are dual aspect and offer a well set out range of rooms with good levels of natural light and they meet all exceed both the relevant internal GIA and exterior amenity space standards for properties of this size. The proposed accommodation is consequently considered acceptable.

7.8 **Parking, servicing and deliveries**

Planning Policy T1 (Strategic approach to transport) of the London Plan 2021 states that the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041. All development should make the most effective use of land, reflecting its connectivity and accessibility by existing and future public transport, walking and cycling routes, and ensure that any impacts on London's transport networks and supporting infrastructure are mitigated.

- 7.8.1 Planning Policy DM T2 (Transport impacts of development) of Merton's Sites and Policies Plans seeks to ensure that development is sustainable and has minimal impact on the existing transport infrastructure and local environment.

Cycle parking

- 7.8.2 Planning Policy T5 (Cycling) of the London Plan 2021 states that development proposals should help remove barriers to cycling and create a healthy environment in which people choose to cycle. Developments should provide cycle parking at least in accordance with the minimum standards set out in Table 10.2. In accordance with Table 10.2, residential dwellings should provide 1 space per studio/1 person 1 bedroom dwelling, 1.5 spaces per 2 person 1 bedroom dwelling and 2 spaces per all other dwellings.
- 7.8.3 A store for the block is proposed to the front of the site to accommodate the required 5 cycles and there is scope for additional space to be provided at the rear of the access path. There would also be tubular mounts for visitor/short term secure facilities.

Car parking

- 7.8.4 Planning Policy T6 (Car parking) of the London Plan 2021 states that Carfree development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Car-free development has no general parking but should still provide disabled persons parking in line with standards set out in policy T6.1 (Residential parking). Planning Policy DM T3 (Car parking and servicing standards) of Merton's Sites and Policies Plan also promotes car free development in areas which benefit from good access to public transport (generally PTAL 4 or above) and be within an existing controlled Parking Zone.
- 7.8.5 The application site has a PTAL rating of 5 (good) and within Controlled Parking Zone RPE. Therefore, the development is considered to be suitable as a permit free development as this would take pressure off existing car parking zones and promote sustainable modes of transport. The applicant has agreed to the permit free requirement, this can be controlled via a S106 agreement restricting permits.

7.9 Bin and Recycling Storage

- 7.9.1 Planning Policy D6 (Housing quality and standards) of the London Plan 2021 states that housing should be designed with adequate and easily accessible storage space that supports the separate collection of dry recyclables (for at least card, paper, mixed plastics, metals, glass) and food waste as well as residual waste.
- 7.9.2 A communal refuse bin storage area is proposed to the front/side of the block, which is considered a suitable and accessible location with refuse facilities of an appropriate size for the development. Therefore the proposal complies with policy CS17(Waste) of the council's Core Strategy Policy and policies SI 7 (Reducing waste and supporting the

circular economy) and SI 8 (Waste capacity and net waste self-sufficiency) of the London Plan 2021 although a condition requiring details of the design to be approved is recommended to ensure suitability of appearance and functionality.

8. SUSTAINABLE DESIGN AND CONSTRUCTION

- 8.1 Merton's Core Planning Strategy (2011) Policy CS15 (Climate change) outlines how all minor and major development, including major refurbishment, should demonstrate: how the proposal makes effective use of resources and materials, minimises water use and CO2 emissions; makes the fullest contribution to minimising carbon dioxide emissions in accordance with the energy hierarchy and designed to withstand the long term impacts of climate change.
- 8.2 London Plan Policies SI 2 (Minimising greenhouse gas emissions) & SI 5 (Water infrastructure) and Merton's Sustainable Design and Construction Explanatory note, expect developments to achieve carbon reductions beyond Part L from energy efficiency measures alone to reduce energy demand as far as possible.
- 8.3 For minor residential developments, development is required to achieve a 19% improvement on Part L of the Building Regulations 2013 and water consumption should not exceed 105 litres/person/day.
- 8.4 The application was submitted with a sustainable design and construction statement outlining how the development has been designed in order to meet these requirements for energy and water savings. It is recommended that a condition be attached requiring details be submitted to demonstrate that a sustainable building has been provided in accordance with relevant policy.

9. Trees

- 9.1 The applications were accompanied by Arboricultural Impact Assessments which confirmed that the proposals would involve the removal of three trees (A Cat B Sycamore, a Cat C Walnut and Cat C Elder) and a shrub (Privet hedge). Some basic tree protection measures and working methodology (in accordance with BS 5837:2012) will ensure the retained and third-party trees and hedges are not detrimentally affected during construction. The Council's Tree Officer has confirmed no objection to the proposal subject to conditions.
- 9.2 Whilst the details would be secured by condition the AIA has suggested the planting of two Silver Birch to the front of the site along with landscaping and another tree to the rear. The new trees will be of standard size (about 2-3m high), planted in full accordance with current British Standards (BS 8545: From Nursery to Independence in the Landscape) and once planted, the trees will be regularly maintained (watered and weeded during the spring and summer months) for at least

5 years or until established. On balance, whilst there would be the removal of three trees, the grade of the trees is low and offers limited amenity value in this instance. In that assessment is the fact that new tree planting can be secured via planning condition.

10. **Other matters**

- 10.1 The existing site is partially open to wildlife but it is not designated as any form of 'green' amenity facility and therefore its loss would not constitute grounds for refusal.
- 10.2 The proposals would impact on the garage of the adjoining neighbour and had the application been recommended for approval it would have been conditioned such that details of how the sub stations could be demolished with no ill effect on that neighbouring structure would be required to be approved and would be subject to the agreement of that neighbour.
- 10.3 The area is at low risk of flooding and therefore development could not be refused on the principle of a development at this location.
- 10.4 Building on land within the demise of Langham Court would be a civil matter between the leaseholders and freeholder and the impact on the value of nearby properties is similarly not a planning matter.
- 10.5 The proposal is for three flats and therefore does not fall subject to the threshold for being subject to affordable housing contributions/provision

11. **CONCLUSION**

- 11.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.
- 11.2 The development of the site would provide new housing for which there is a recognised need. The block of three flats is considered to be well and attractively designed and exceed the minimum requirements for internal and external space. The bulk scale, massing and siting of the block is considered to be of an acceptable appearance and not to have a negative impact on neighbour amenity.
- 11.3 There are considered to be no justifiable reasons to refuse the application on the grounds of fire safety and subject to the provision of an alternative electricity substation prior to the demolition of the existing buildings the proposals will not negatively impact utility supplies.
- 11.4 The site is ideally located in Raynes Park town centre with its local facilities and very good public transport connections and therefore is

suitable for being a permit free development in order to reduce reliance on the car and to increase pressure on parking locally.

- 11.5 The proposal are therefore considered to be in accordance with Adopted Sites and Policies Plan, Core Planning Strategy and London Plan policies. The proposal is therefore recommended for approval subject to conditions and S106 agreement.

12. RECOMMENDATION

GRANT PLANNING PERMISSION subject to –

1. Subject to conditions and the completion of a Section 106 Agreement covering the following heads of terms:-

1. Permit Free.
2. The developer agreeing to meet the Councils costs of preparing, drafting and monitoring the Section 106 Obligations.

And the following conditions:

1. A1 Commencement of Development
2. A7 Approved Plans; Site location plan and drawings, 1158P- 20, 200, 201, 210, 211, 212 & 213
3. No development shall commence until alternative electrical substation facilities, as confirmed by UK Power Networks, to serve the local area have been provided and made fully operational before any works have commence on site.

Reason, to ensure the continuous provision of electricity utilities in accordance with policy CS11 of the Merton Core Strategy 2011.

4. B1 External Materials to be Approved – No development, other than demolition, shall take place until details of particulars and samples of the materials to be used on all external faces of the development hereby permitted, including window frames and doors (notwithstanding any materials specified in the application form and/or the approved drawings), have been submitted to the Local Planning Authority for approval. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall be carried out in full accordance with the approved details.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for

Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

5. C07 Refuse & Recycling – No development shall be occupied until details a refuse management plan for the management and collection of waste on site have been submitted in writing for approval to the Local Planning Authority.

Reason: To ensure the provision of satisfactory management methods for the storage and collection of refuse and recycling material and to comply with the following Development Plan policies for Merton: policies SI 7 and D6 of the London Plan 2021, policy CS17 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014.

6. D10 External Lighting – Any external lighting shall be positioned and angled to prevent any light spillage or glare beyond the site boundary.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies DM D2 and DM EP4 of Merton's Sites and Polices Plan 2014.

7. D11 Construction Times – No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policy D14 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Polices Plan 2014.

8. Non-standard condition (Demolition and Construction Method Statement) – No development shall take place until a Demolition and Construction Method Statement has been submitted to, and approved in writing by, the Local planning authority. The approved Statement shall be adhered to throughout the demolition and construction period.

The Statement shall provide for:

- hours of operation
- the parking of vehicles of site operatives and visitors
- loading and unloading of plant and materials
- storage of plant and materials used in constructing the development

- the erection and maintenance of security hoarding including decorative -displays and facilities for public viewing, where appropriate
- wheel washing facilities
- measures to control the emission of noise and vibration during construction.
- measures to control the emission of dust and dirt during construction/demolition
- a scheme for recycling/disposing of waste resulting from demolition and construction works

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

9. H13 Construction Logistics Plan to be submitted – Prior to the commencement of the development hereby permitted, a Demolition/Construction Logistics Plan (including a Construction Management plan in accordance with TfL guidance) shall be submitted to and approved in writing by the Local Planning Authority. The approved Statement shall be adhered to throughout the construction period unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

10. F1 No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved before the commencement of the use or the occupation of any building hereby approved, unless otherwise agreed in writing by the Local Planning Authority. The details shall include on a plan, full details of the size, species, spacing, quantities and location of proposed plants, together with any hard surfacing, means of enclosure, and indications of all existing trees, hedges and any other features to be retained, and measures for their protection during the course of development.

11. F5 No development [including demolition] pursuant to this consent shall commence until an Arboricultural Method

Statement and Tree Protection Plan, drafted in accordance with the recommendations and guidance set out in BS 5837:2012 has been submitted to and approved in writing by the Local Planning Authority and the approved details have been installed. The details and measures as approved shall be retained and maintained, until the completion of all site operations.

12. F8 Site supervision: The details of the Arboricultural Method Statement and Tree Protection Plan shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.
13. B4 No development shall take place until details of the surfacing of all those parts of the site not covered by buildings or soft landscaping, including any parking, service areas or roads, footpaths, hard and soft have been submitted in writing for approval by the Local Planning Authority. No works that are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details have been approved and works to which this condition relates have been carried out in accordance with the approved details.
14. C5 No cables, wires, aerials, pipe work (except any rainwater down pipes as may be shown on the approved drawings) meter boxes or flues shall be fixed to any elevation facing a highway.
15. B5 No development shall take place until details of all boundary walls or fences are submitted in writing for approval to the Local Planning Authority. No works which are the subject of this condition shall be carried out until the details are approved, and the development shall not be occupied / the use of the development hereby approved shall not commence until the details are approved and works to which this condition relates have been carried out in accordance with the approved details. The walls and fencing shall be permanently retained thereafter.
16. H07 Cycle Parking – No development shall not be occupied until the secure cycle parking facilities for the occupants of, and visitors to, the development shown on the approved drawings have been fully implemented and made available for use prior

to the first occupation of the development and thereafter retained for use at all times.

Reason: To ensure satisfactory facilities for cycle parking are provided and to comply with the following Development Plan policies for Merton: policy T5 of the London Plan 2021, policy CS18 of Merton's Core Planning Strategy 2011 and policy DM T1 of Merton's Sites and Policies Plan 2014.

17. Nonstandard condition; Sustainable build; The residential units hereby approved shall not be occupied until it has demonstrated to and approved in writing by the Local Planning Authority that they have achieved an energy efficiency standard of a minimum of 19% CO2 improvement over Building Regulations requirements Part L 2013 (TER Baseline) and, as a minimum, a water efficiency standard of not more than 110 litres per person per day maximum water consumption (to include a fixed factor of water for outdoor use of 5 litres per person per day in accordance with the optional requirement defined within Approved Document G of the Building Regulations).

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply with the following Development Plan policies for Merton: policy SI 2 of the London Plan 2021 and policy CS15 of Merton's Core Planning Strategy 2011.

18. C9 The screening or enclosure to the balconies as shown on the approved plans shall be implemented before the development is first occupied and retained permanently thereafter.
19. B6 No development shall take place until details of the proposed finished floor levels of the development, together with existing and proposed site levels, have been submitted to and approved in writing by the Local Planning Authority, and no development shall be carried out except in strict accordance with the approved levels and details.
20. F9 The hardstanding hereby permitted shall be made of porous materials, or provision made to direct surface water run-off to a permeable or porous area or surface within the application site before the development hereby permitted is first occupied or brought into use.
21. C6 No development shall take place until a scheme for the storage of refuse and recycling has been submitted in writing for approval to the Local Planning Authority. No works which

are the subject of this condition shall be carried out until the scheme has been approved, and the development shall not be occupied until the scheme has been approved and has been carried out in full. Those facilities and measures shall thereafter be retained for use at all times from the date of first occupation.

24. No development (other than demolition) approved by this permission shall be commenced until a scheme for the provision of surface and foul water drainage has been implemented in accordance with details that have been submitted to and approved in writing by the LPA. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system (SuDS) to ground, watercourse or sewer in accordance with drainage hierarchy contained within the London Plan Policy SI.3 and the advice contained within the National SuDS Standards. Where a sustainable drainage scheme is to be provided, the submitted details shall:
- i. provide information about the design storm period and intensity, the method employed to delay (attenuation provision of no less than 13m³) and control the rate of surface water discharged from the site to greenfield runoff rates (no more than 5l/s), and the measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - ii. include a timetable for its implementation; and
 - iii. provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption authority and any other arrangements.

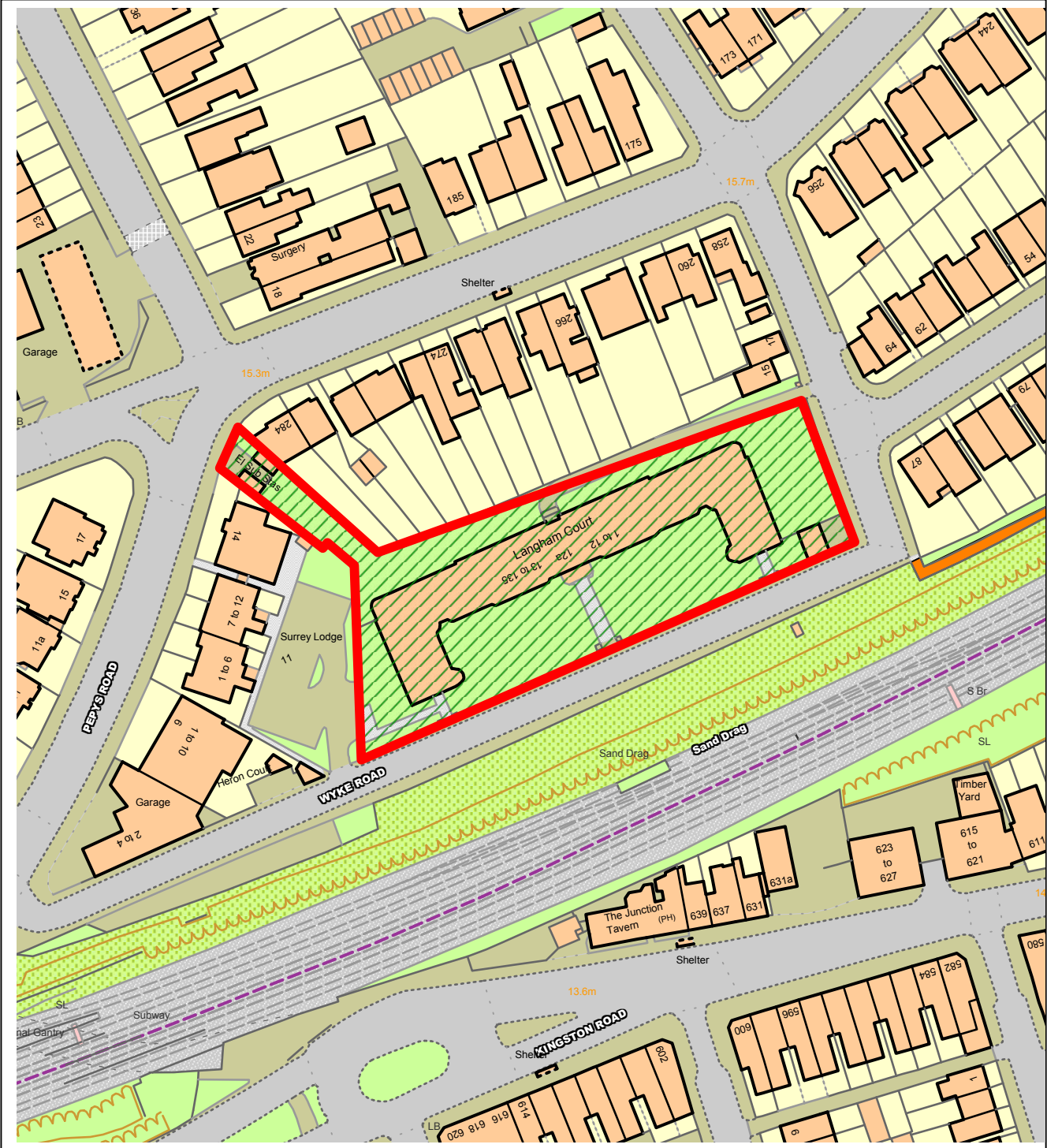
Reason: To reduce the risk of surface and foul water flooding and to ensure the scheme is in accordance with the drainage hierarchy of London Plan policy SI.3 and the National SuDS standards and in accordance with policies CS16 of the Core Strategy and DMF2 of the Sites and Policies Plan.

25 Green Roofs/Walls

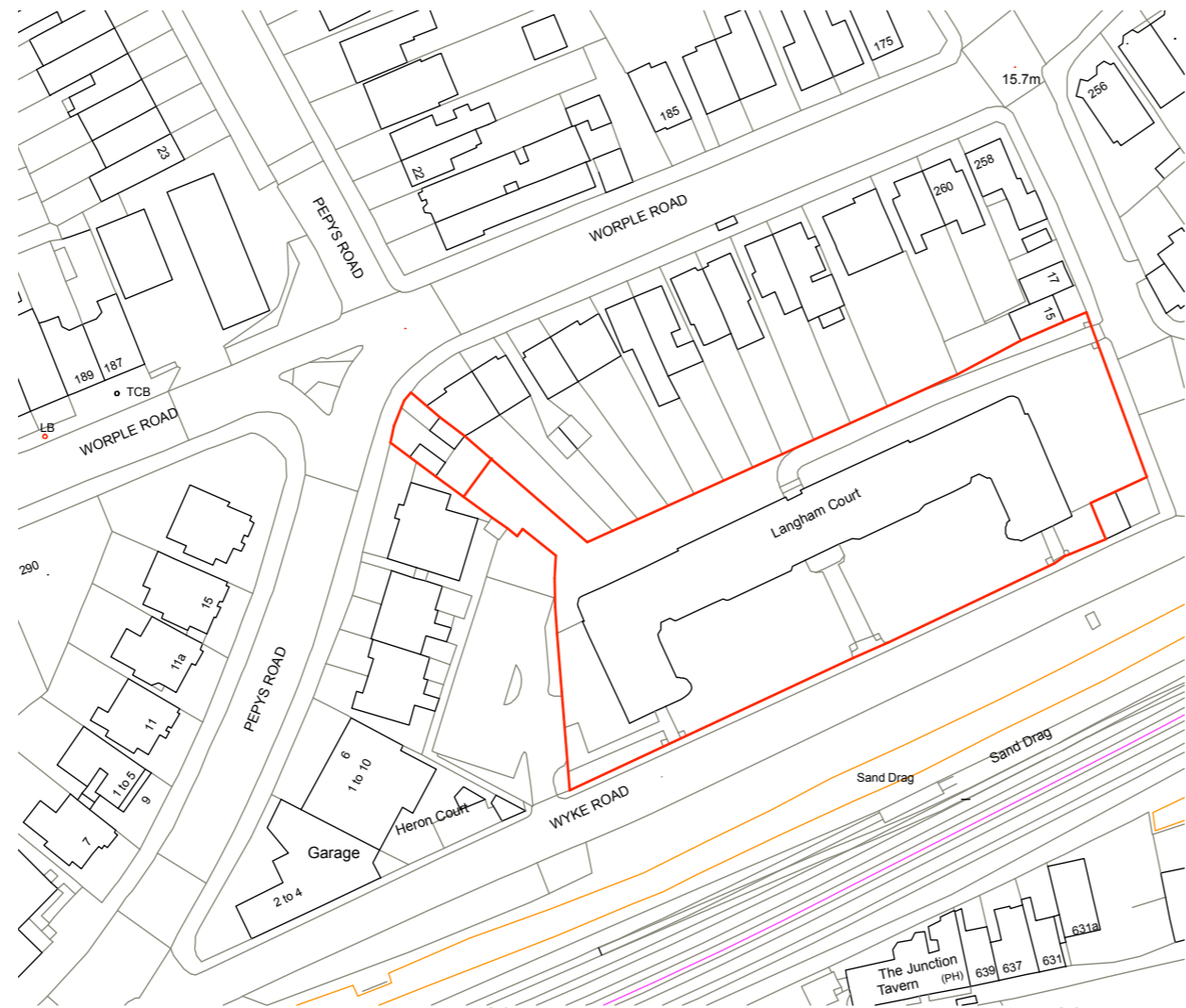
Prior to commencement of above ground works details for the proposed green roof and walls shall be submitted to an approved in writing by the Local Planning Authority and those approved measures shall be installed in the planting season following construction of the development.

Reason: To ensure the provision of an attractive and viable green roof and wall system to improve biodiversity and greening of the development in accordance with Merton sites and Policies Plan policies DM O2 and DM D2.

NORTHGATE SE GIS Print Template

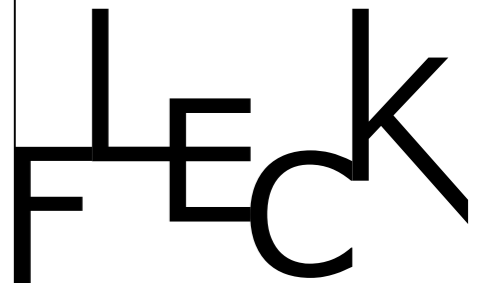


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24 Godwin Road, London E7 0LE
 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:

A Azouz

PROJECT:

Substation site flats, Pepys Road
 SW20

STATUS:

PLANNING

DRAWING:

Site location plan

DRAWN BY: DATE:

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pedestrian access route for Langham Court paving with permeable joints laid to fall toward planting areas

bicycle store with green roof and electric charging point

planting areas containing native planting helping to slow and reduce surface water run off and improve biodiversity of site

paving with permeable joints laid to fall toward planting area/ gravel

semi detached house at 284 Worple Road

garage belonging to 284 Worple Road

paving with permeable joints laid to fall toward planting areas

office building at 14 Pepys Road

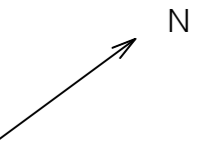
existing garden space unaffected by the proposals

existing planting along boundary with 284 Worple Road to be retained

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(WB) rainwater harvesting (water butt)



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T: 07814734654 E: info@fleckarchitecture.com

CLIENT:

A Azouz

PROJECT:

Substation site flats, Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed block plan

DRAWN BY: DATE:

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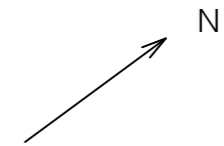
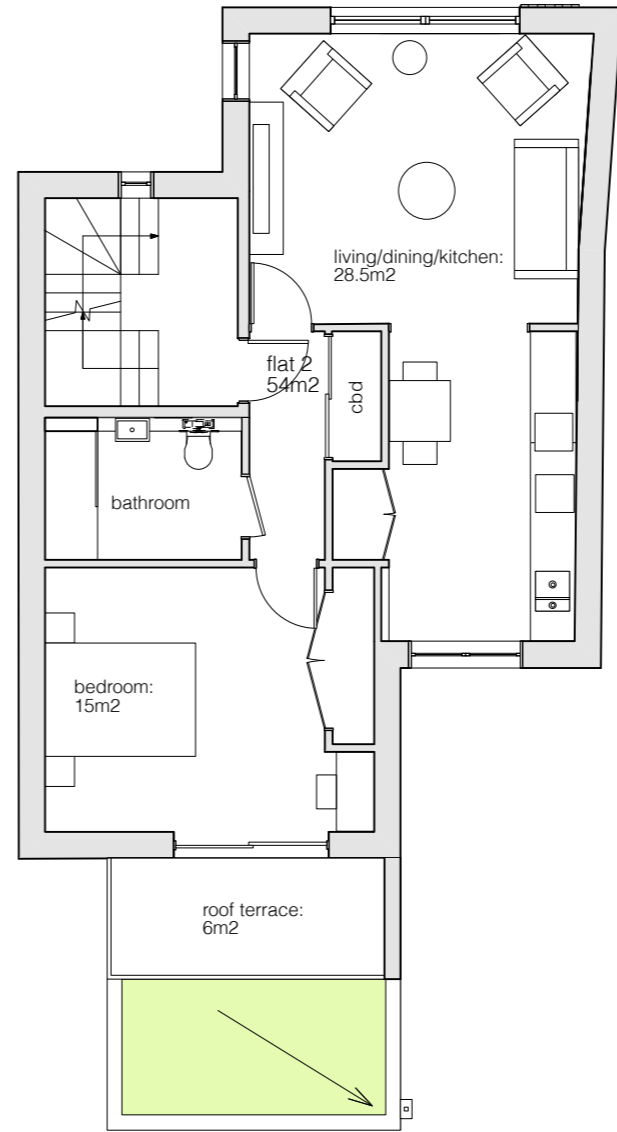
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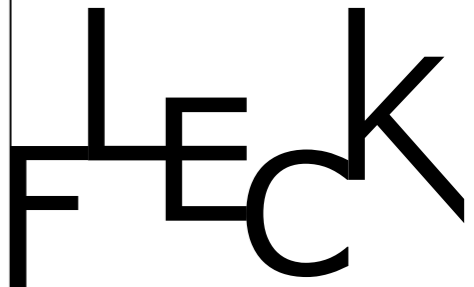
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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:
 A. Azouz

PROJECT:
 Proposed flats,
 Pepys Road SW20

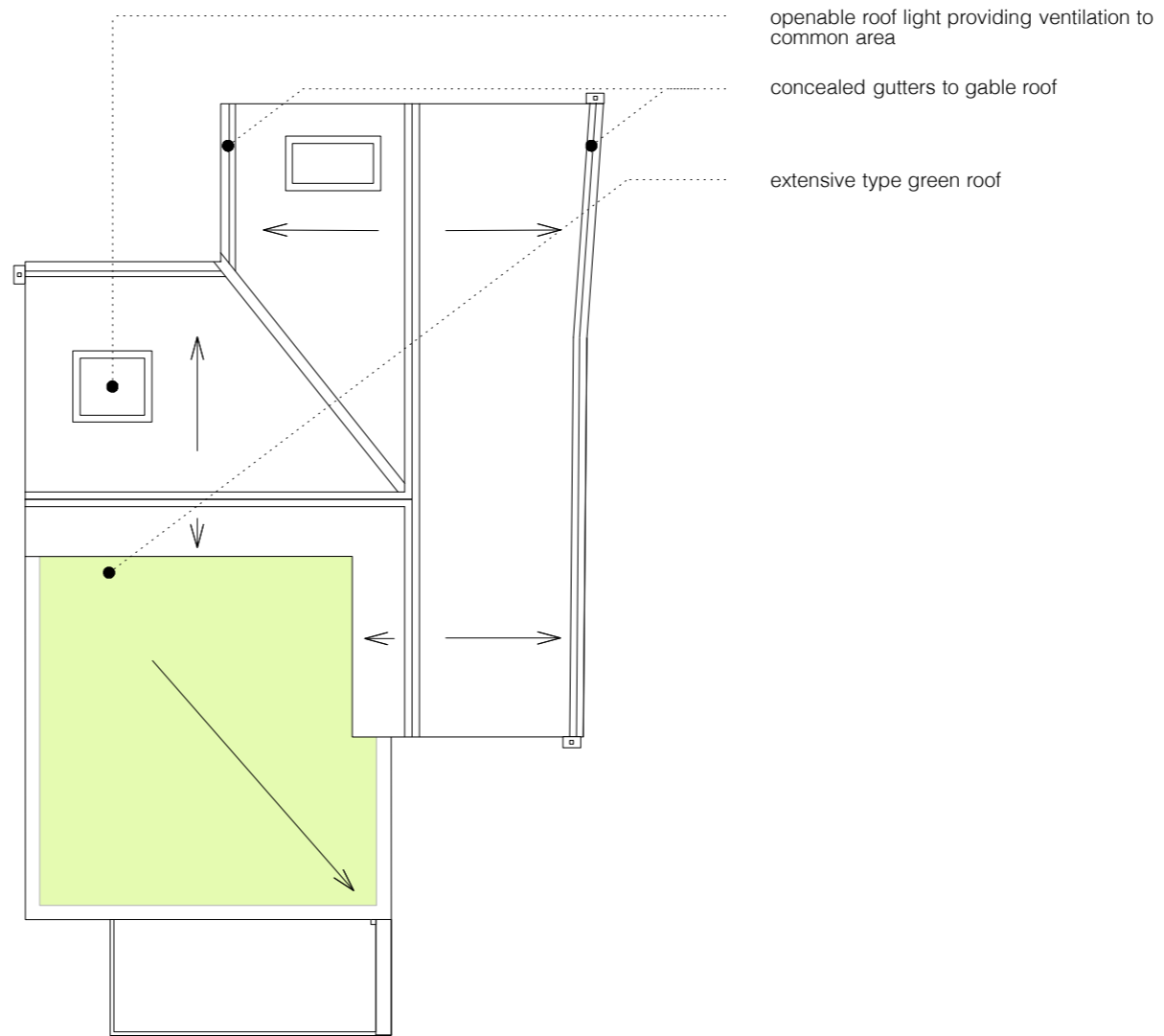
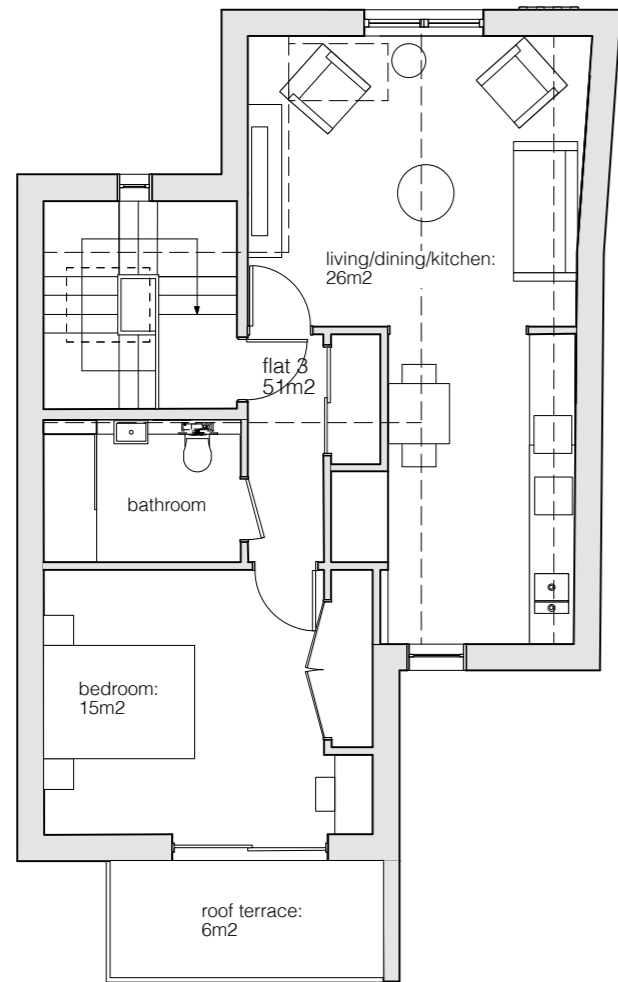
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PLANNING

DRAWING:
 Proposed ground and 1st floor plan

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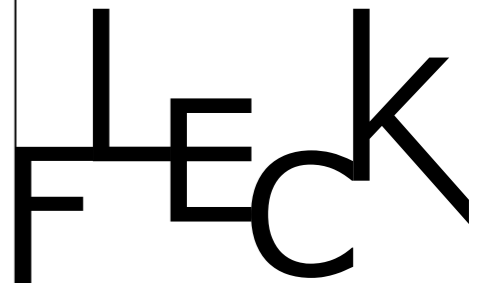
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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:

A. Azouz

PROJECT:

Proposed flats,
 Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed 2nd floor and roof plan

DRAWN BY: DATE:

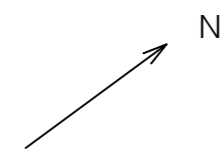
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284 Worple Road

Substation site

14 Pepys Road



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T: 07814734654 E: info@fleckarchitecture.com

CLIENT:

A. Azouz

PROJECT:

Proposed flats,
Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed front elevation

DRAWN BY:

C F

DATE:

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SCALE:

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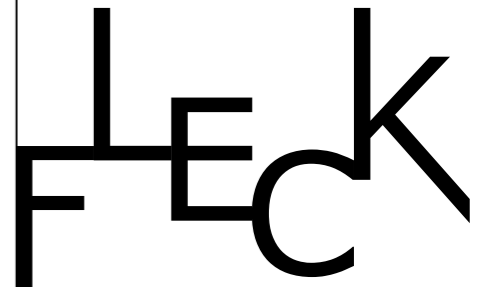




green wall formed of climbers rooted in soil

NOTES:

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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:

A. Azouz

PROJECT:

Proposed flats,
Pepys Road SW20

STATUS:

PLANNING

DRAWING:

Proposed rear elevation

DRAWN BY: DATE:

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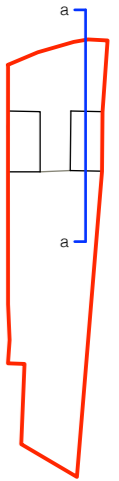




sectional elevation a-a

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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:
 A. Azouz

PROJECT:
 Proposed flats,
 Pepys Road SW20

STATUS:
PLANNING

DRAWING:
 Proposed sectional elevation

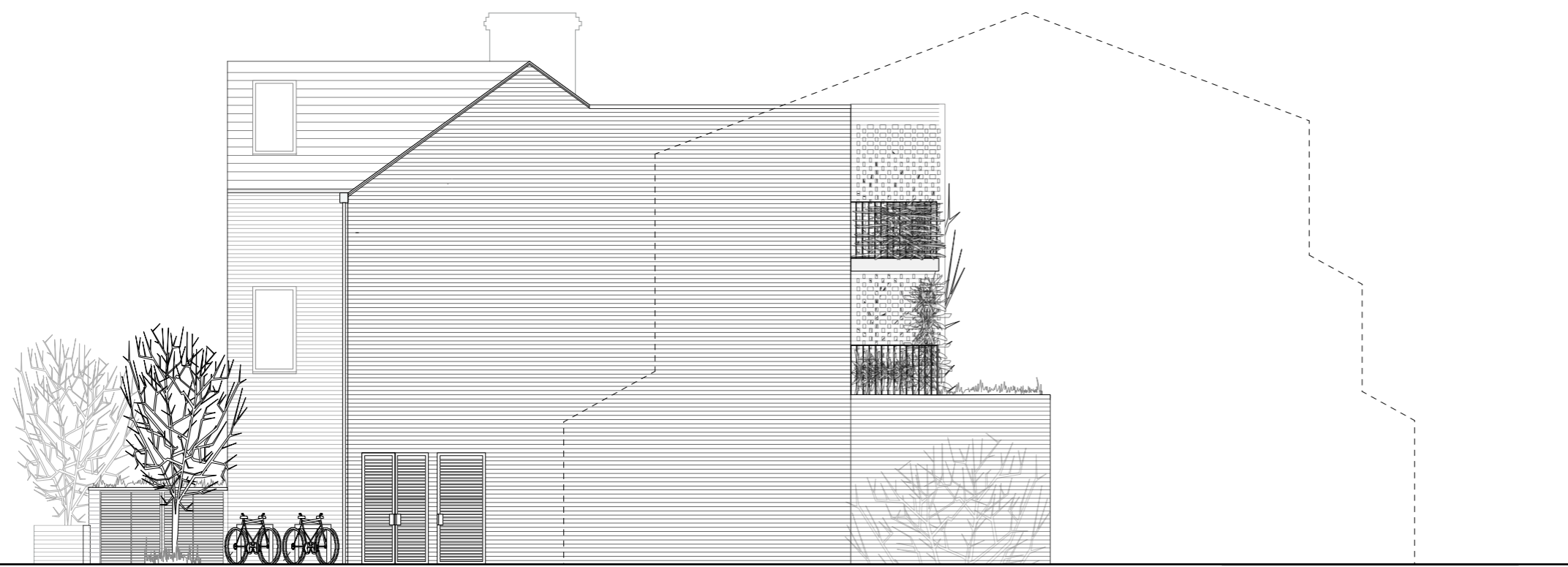
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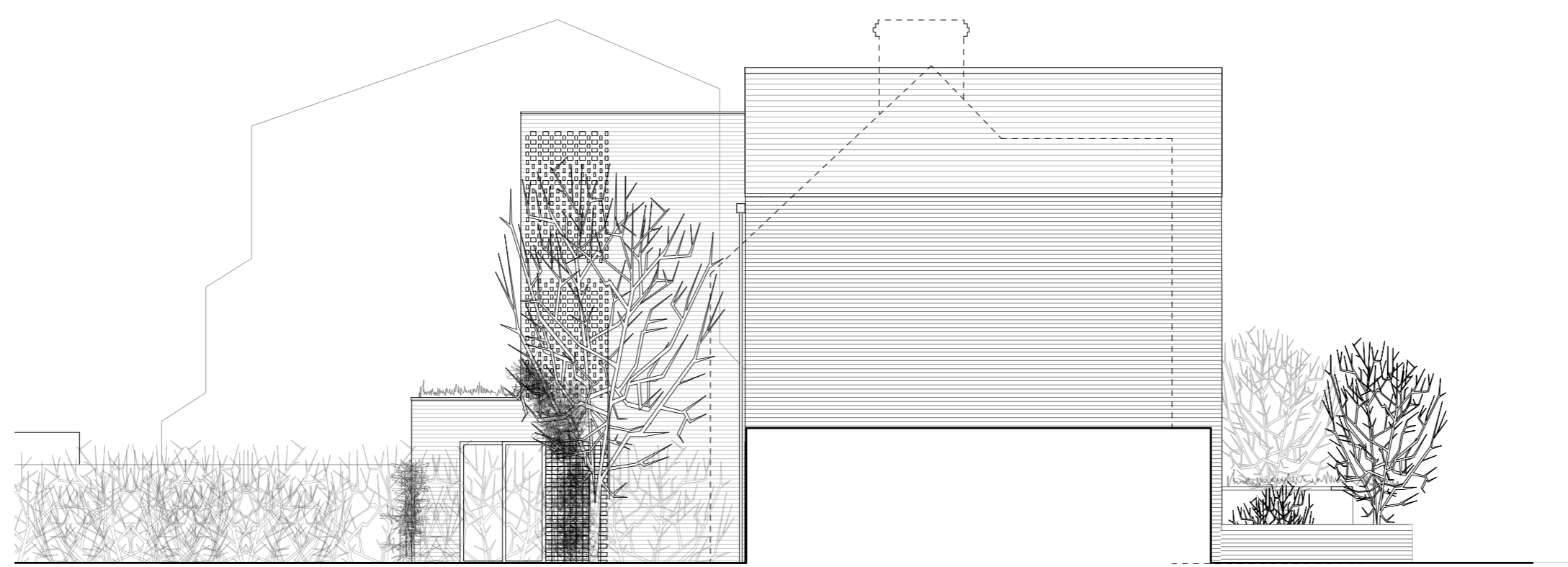
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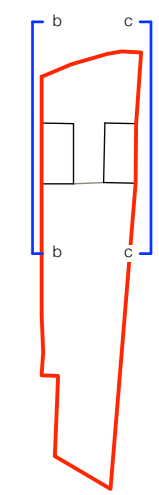
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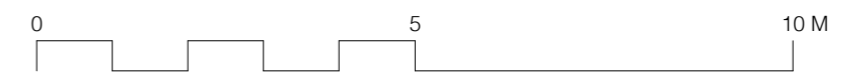
south west side elevation b-b



north east side elevation c-c



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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT:
 A. Azouz

PROJECT:
 Proposed flats,
 Pepys Road SW20

STATUS:
PLANNING

DRAWING:
 Proposed side elevations

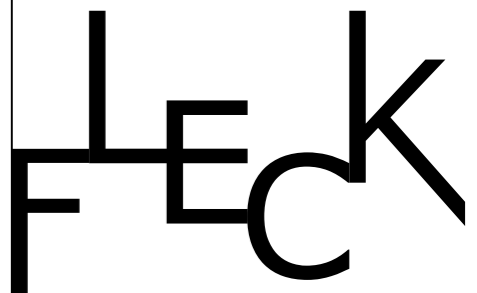
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 T: 07814734654 E: info@fleckarchitecture.com

CLIENT: _____
 A Azouz
 PROJECT: _____
 Pepys Road SW20

STATUS: _____
PRE-AP CONSULTATION

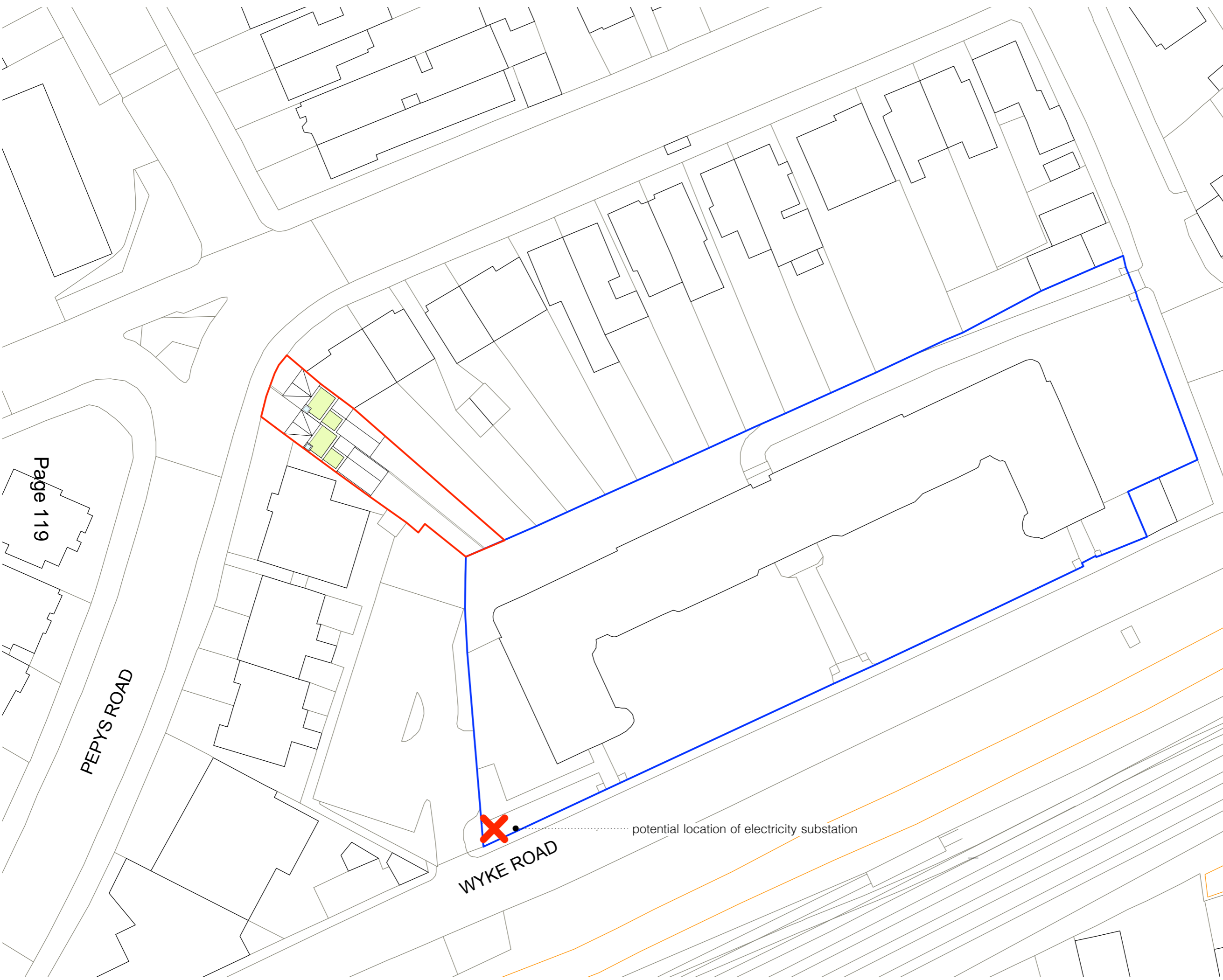
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 SITE LOCATION PLAN

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SCALE: _____
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Page 119

PEPYS ROAD

WYKE ROAD

potential location of electricity substation

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